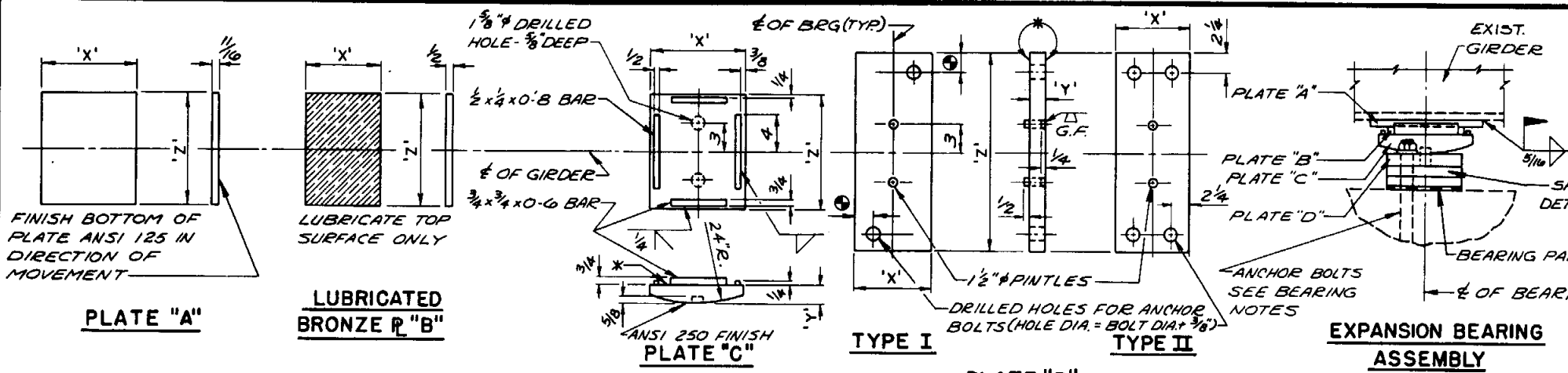


TABLE OF FILLET WELD SIZES

THICKNESS OF THICKER PART JOINED	# MIN SIZE OF FILLET WELD
TO 3/4" INCLUSIVE	1/4"
OVER 3/4" TO 1 1/2"	3/16"
OVER 1 1/2" TO 2 1/4"	3/8"
OVER 2 1/4" TO 6"	1/2"

*EXCEPT THAT WELD SIZE SHALL NOT EXCEED THICKNESS OF THINNER PART JOINED



BEARING NOTES

ALL BEARINGS ARE SYMMETRICAL ABOUT ϵ OF GIRDER AND ϵ OF BEARING.

ALL STRUCTURAL STEEL BEARING PLATES SHALL BE FLAT ROLLED STEEL PLATES WITH ALL SURFACES SMOOTH AND FREE FROM WARP AND ALL EDGES SMOOTH, STRAIGHT AND VERTICAL.

ALL PLATE CUTS SHALL BE MACHINE OR MACHINE FLAME CUTS.

ALL FINISHED SURFACES SHALL BE MACHINE FINISHED BY AN AUTOMATIC PROCESS.

LOCATE ANCHOR BOLTS AS INDICATED FOR PLATE "D". SEE TABLE FOR SIZE. BOLT LENGTH TO BE 1-5 FOR 1 1/2" ϕ AND 1-10 FOR 1 1/4" ϕ BOLTS. PROJECT ANCHOR BOLTS 1 1/2" ABOVE TOP OF CONCRETE.

ANCHOR BOLTS SHALL BE THREADED 3". PROVIDE ONE STANDARD WROUGHT WASHER AND ONE HEX. NUT PER BOLT. FOR UNPAINTED STRUCTURES THE UPPER 6 INCHES OF THE ANCHOR BOLTS, NUTS AND WASHERS SHALL BE GALVANIZED AS REQUIRED BY ASTM DESIGNATION A153, CLASS C OR A164, TYPE G5.

CHAMFER TOP OF PINTLES 1/8". DRILL HOLES FOR PINTLES IN ALL "D" PLATES FOR DRIVING FIT.

ALL MATERIAL INCLUDING SHIMS BUT EXCLUDING ANCHOR BOLTS, PINTLES, NUTS AND WASHERS SHALL BE MADE OF A 588 STEEL. PINTLES SHALL BE MADE OF A449 STEEL.

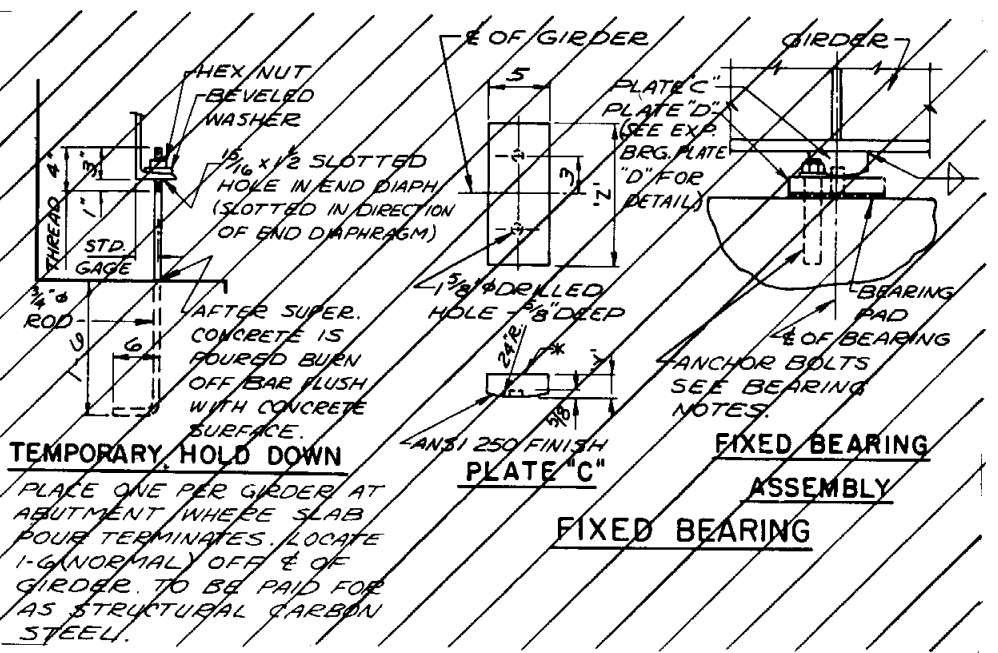
ALL MATERIAL IN BEARINGS, INCLUDING BRONZE PLATES AND BEARING PADS SHALL BE PAID FOR AT THE UNIT PRICE BID FOR "EXPANSION BEARING REPLACEMENT". PROVIDE 3/8" THICK BEARING PAD SAME SIZE AS PLATE "D" FOR EACH BEARING.

EXPANSION BEARING

	PLATE "A"		PLATE "B"		PLATE "C"		PLATE "D"			PLATE "D" TYPE	ANCHOR BOLT SIZE	NO. OF BRG'S. REQ'D.	LOCATION	
	X	Z	X	Z	X	Y	Z	X	Y					Z
EXPANSION BEARING	9"	1'-0"	5"	1'-0"	7"	1 1/8"	1'-2 1/4"	8"	1 1/2"	1'-10"	I	1 1/4" ϕ	6	W. ABUT.
FIXED BEARING														

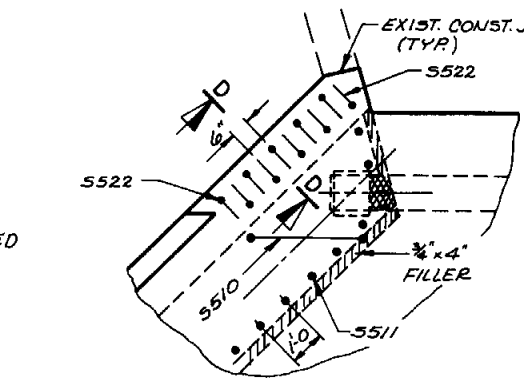
2" WHEN 1 1/4" ϕ ANCHOR BOLTS ARE USED AND 2 1/4" WHEN 1 1/2" ϕ ANCHOR BOLTS ARE USED.

* FINISH ANSI 250 IF DIMENSION 'Y' IS GREATER THAN 2".

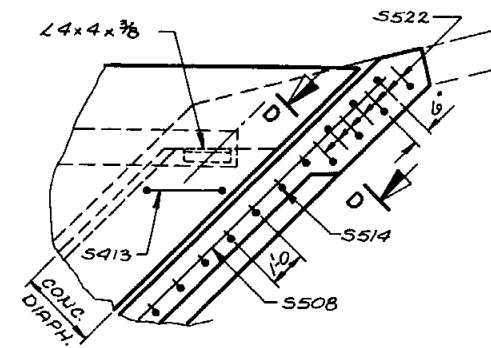


TEMPORARY HOLD DOWN
 PLACE ONE PER GIRDER AT ABUTMENT WHERE SLAB POUR TERMINATES. LOCATE 1'-6" NORMAL OFF ϵ OF GIRDER. TO BE PAID FOR AS STRUCTURAL CARBON STEEL.

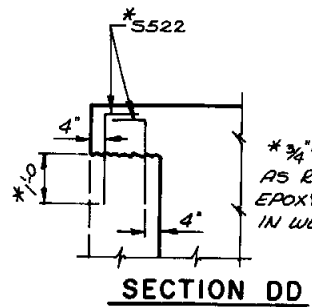
FIXED BEARING ASSEMBLY
 ANCHOR BOLTS SEE BEARING NOTES.



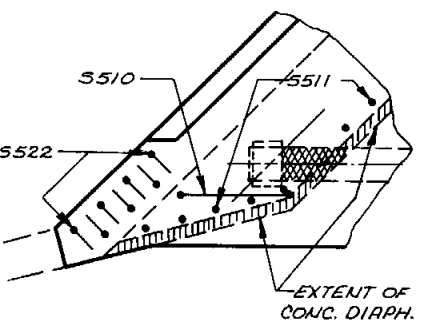
EAST ABUT.



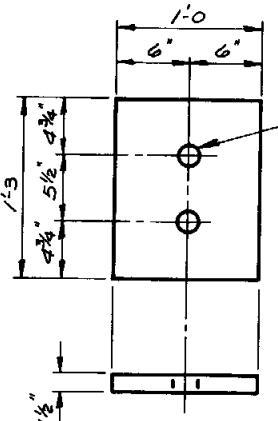
WEST ABUT.



SECTION DD

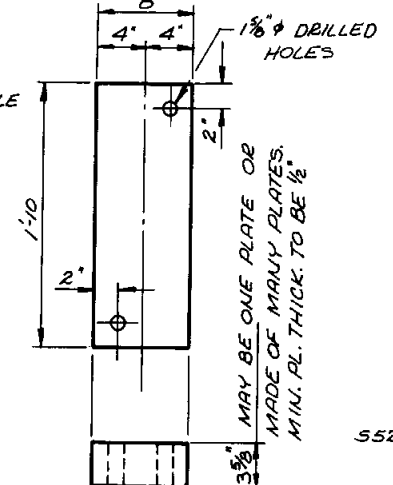


CORNER DETAILS



NEW TOP PLATE (1 REQ'D.)

NOTE: NEW TOP PLATE REQ'D. AT GIRDER #4 - E. ABUT.



SHIM PLATE (6 REQ'D.)

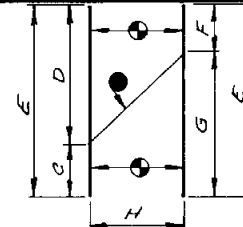
No.	Date	Revision	By
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS			
STRUCTURE B-52-822			
Const. Spec.	1981	Drawn By	R. J. G.
		Plans Checked	J. W.
BEARING DETAILS			SHEET 2 OF 4
			X68622

BILL OF BARS

MARK	NO. REQ'D.	LENGTH	BENT	CUT. DIA.	LOCATION
2	S501	145	17-8		SLAB-TOP#BOT-TRANS.
3	S502	152	15-7		" " " "
1	S403	98	3-0		" " " -TRANS.
1	S504	22	17-5	*	" " " -SETS 1#2
1	S505	22	17-6	*	" " " -SETS 3#4
4	S406	130	32-5		" -TOP & BOT - LONG.
1	S507	6	22-0		ABUTS - BACKWALL
1	S508	6	24-1		" " " "
1	S609	4	22-0		E. & W. ABUT. DIAPH.
1	S510	26	13-11	*	W. ABUT.
1	S511	37	2-0		" " " "
1	S612	25	7-3		E. & W. ABUT. DIAPH.
1	S413	30	7-5	*	W. ABUT.
1	S514	37	3-0	*	" " " "
1	S615	20	10-0	*	RAIL POST BASE
1	S616	44	4-0		" " " "
1	S517	1	1-9		SLAB-TOP-TRANS.
5	S518	25	19-9	*	" -TOP & BOT-TRANS-SETS 5#6
6	S519	26	19-4	*	" " " " " -SETS 7#8
1	S620	4	24-5		E. ABUT. DIAPH.
1	S621	4	8-3	*	RAIL POST EDGE-CORNERS
1	S522	34	3-0	*	ABUT. BACKWALL

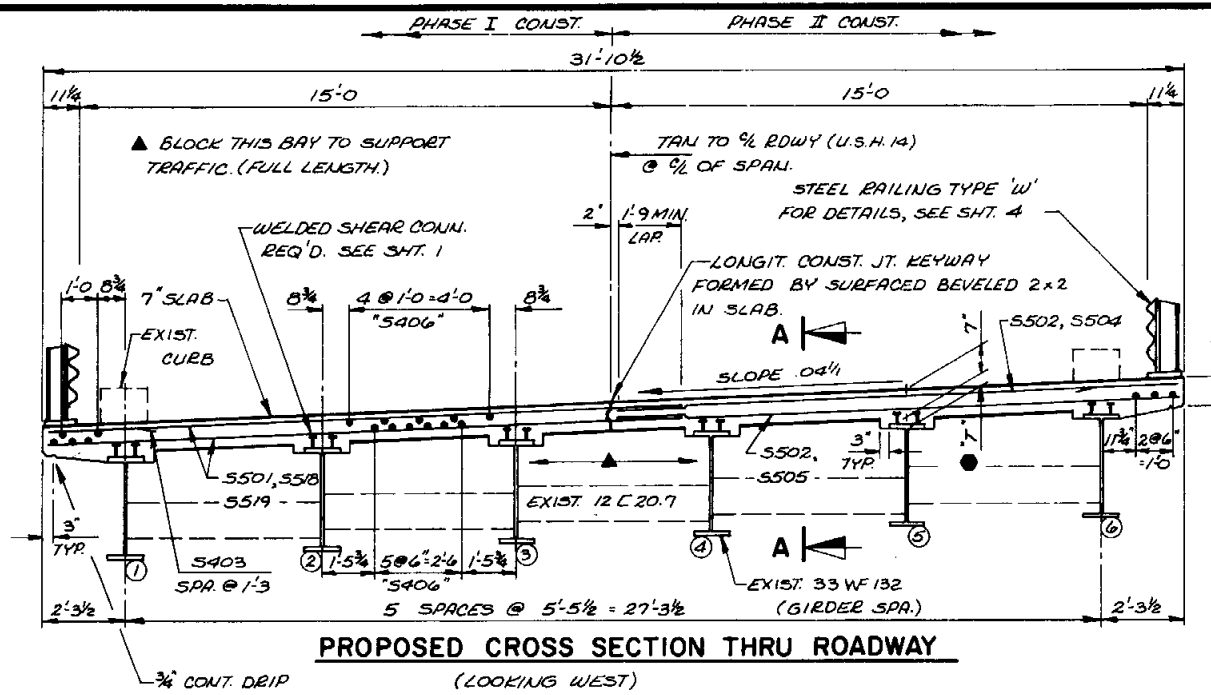
MARK	C	D	E	F	G	H	SETS REQ'D.
S504	SET 1	2-4			15-5		1
"	" 2		15-1	17-5	2-0	22	1
S505	" 3	2-1			15-2	22	1
"	" 4		15-5	17-6	2-4		1
S518	" 5	2-3			17-2	25	1
"	" 6		17-6	19-9	2-7		1
S519	" 7	1-9			17-4	26	1
"	" 8		17-7	19-4	2-0		1

CUTTING DIAGRAM

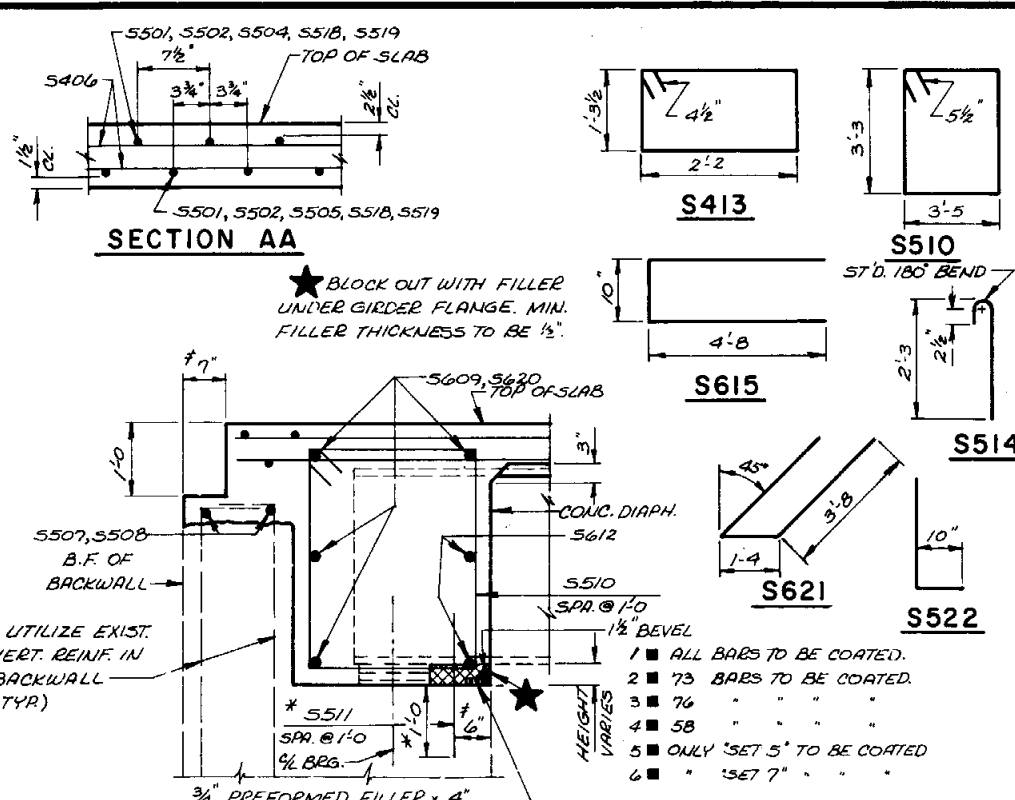


MARK & CUT ALL BARS ALONG THIS LINE. MAKE ALL CUTS NORMAL TO BAR AXIS.
 BUNDLE & MARK OUT BARS WITH BAR # SET NUMBERS.
 'H' IS THE NUMBER OF BARS IN CUTTING DIAGRAM BEFORE CUTTING.

NOTE: THE FIRST DIGIT OF A BAR MARK SIGNIFIES THE BAR SIZE.



PROPOSED CROSS SECTION THRU ROADWAY (LOOKING WEST)



SECTION AA

SECTION BB

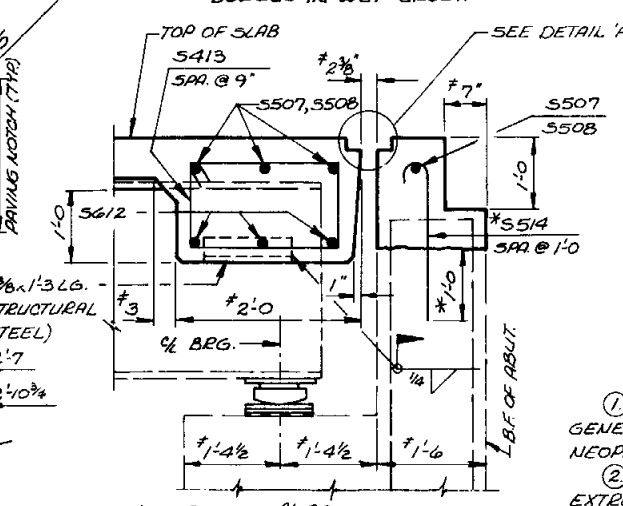
NOTE:
 1 ALL BARS TO BE COATED.
 2 73 BARS TO BE COATED.
 3 76 " " " "
 4 58 " " " "
 5 ONLY 'SET 5' TO BE COATED
 6 " 'SET 7' " " "

JOINT NOTES

ONE FIELD SPLICE REQ'D. IN STEEL EXTRUSIONS. DETAILS SHALL BE SUBMITTED FOR APPROVAL.
 NO SPLICE PERMITTED IN NEOP. STRIP SEAL. SAND BLAST CLEAN STEEL EXTRUSIONS PRIOR TO COATING WITH LUBRICANT ADHESIVE FOR NEOPRENE SEAL.
 STRIP SEAL EXP. JT. ASSEMBLY, INCLUDING ANCHOR STUDS & HARDWARE WILL BE PAID FOR AT THE LUMP SUM PRICE BID FOR "EXPANSION DEVICE."

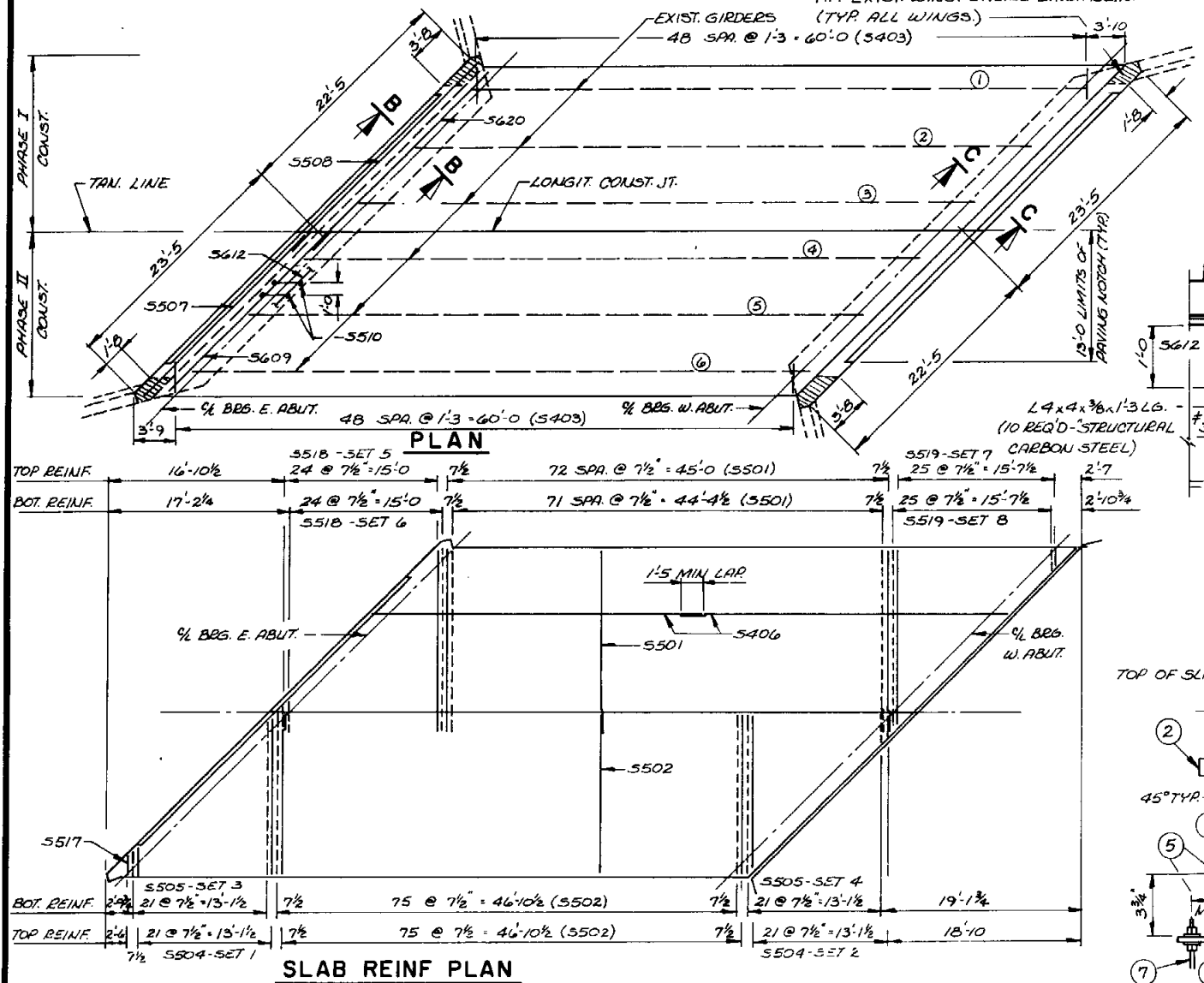
LEGEND

- ACME-ACMA MODEL AS-300A, D.S. BROWN 55-300, GENERAL TIRE G5-300A, OR WATSON-BOWMAN MODEL 5-300(A-2) NEOPRENE STRIP SEAL & STEEL EXTRUSIONS TO LIMITS SHOWN.
- STUDS 3/8" x 0'-6 3/8" LONG @ 6" ALT. CTRS. WELD TO STEEL EXTRUSIONS ① & BEND AS SHOWN.
- PL. 3/8" x 4" WIDE WITH 1 1/2" HOLE FOR ④. BEND & WELD TO ① @ 3'-0" CTRS.
- ROD 3/4" x 1'-6" WITH 2 STD. NUTS & WASHERS. THRD ONE END 4" & EMBED IN CONC. AT 3'-0" CTRS. (FIELD DRILLED & GROUTED.)
- FABRICATE SUPPORT FROM 1/2" x 3" BAR OR EQUIVALENT, ONE PER GIRDER. WELD TO ①. PROVIDE 1 1/2" HOLE FOR ⑦ & 1" HOLE FOR ⑥.
- 3/4" THREADED ROD WITH NUT. TACK WELD NUT TO ⑤.
- SUPPORT ROD 3/4" WITH 2 NUTS & WASHERS. WELD THREADED ROD TO TOP FLG. OR ATTACH BY BOLTING THRU FLG.



SECTION CC

DETAIL 'A'



SLAB REINF PLAN

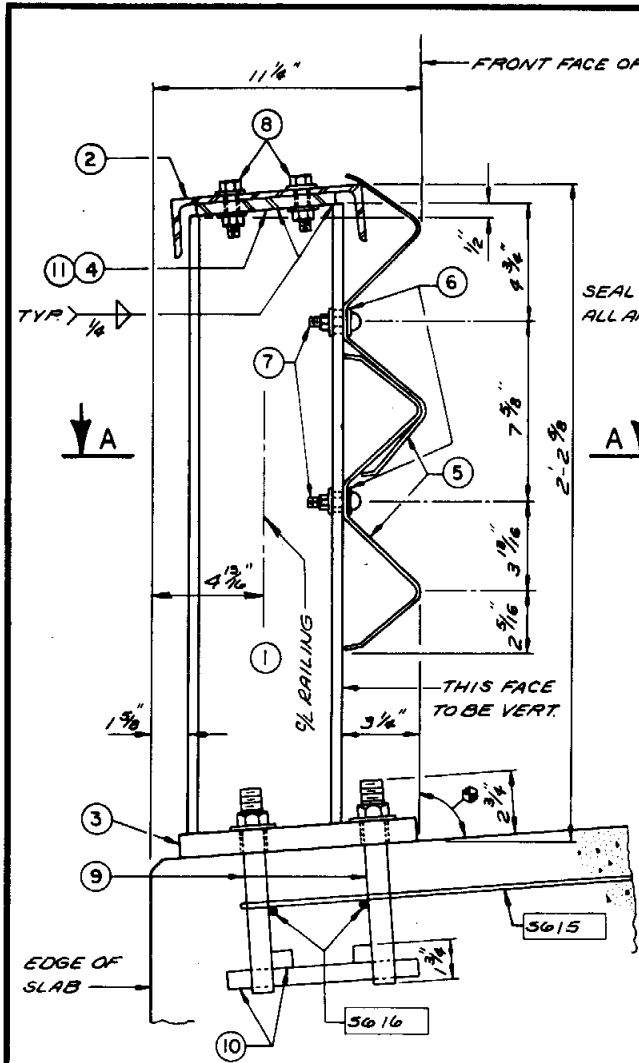
TO DETERMINE T AFTER ALL CONC. HAS BEEN REMOVED, ELEV'S OF THE EXIST. TOP FLANGES SHALL BE TAKEN AT THE 1/4 POINTS. THESE ELEV'S. SUBTRACTED FROM THE GRADE ELEV'S. ADJUSTED FOR DEAD LOAD DEFLECTIONS MINUS THE SLAB DEPTH, PLUS THE STEEL THICK. TO BOT. OF THE TOP FLANGE EQUALS THE HAIRING DEPTH 'T'.

REMOVE EXIST. 1" FILLER & BACKWALL TO F.F. EXIST. WING. UTILIZE EXIST. REINF. (TYP. ALL WINGS)

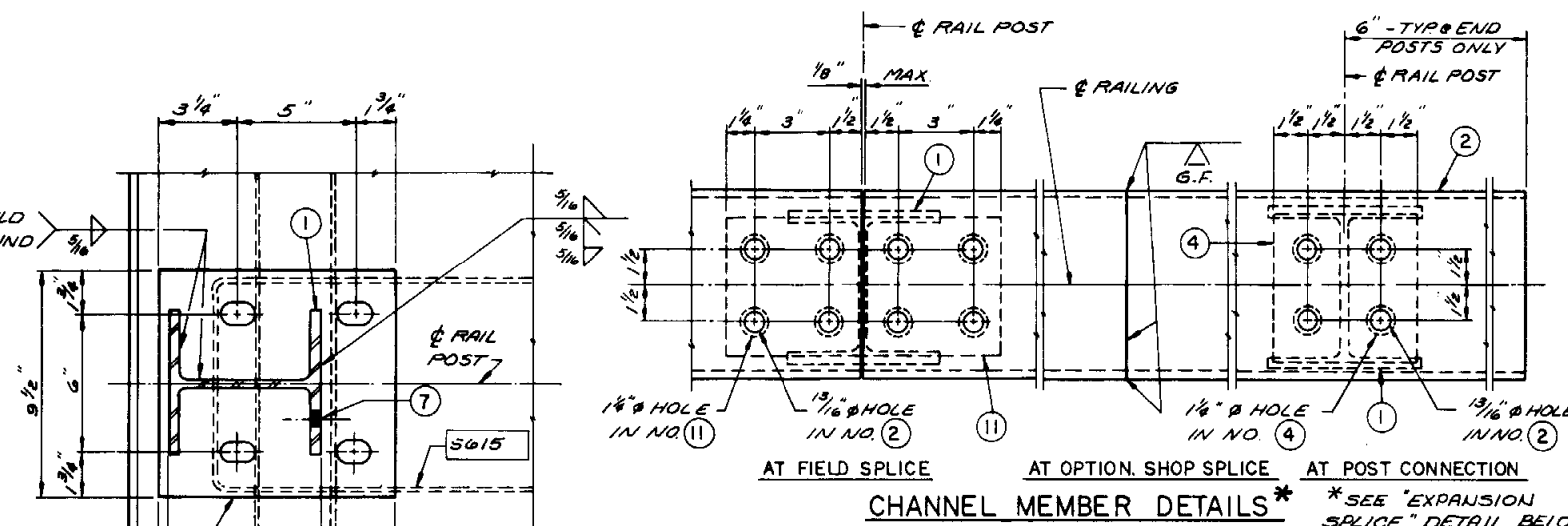
* 3/4" DRILLED HOLE. FILL AS REQ'D. WITH POURABLE EPOXY GROUT, SET DOWELS IN WET GROUT.

No.	Date	Revision	By

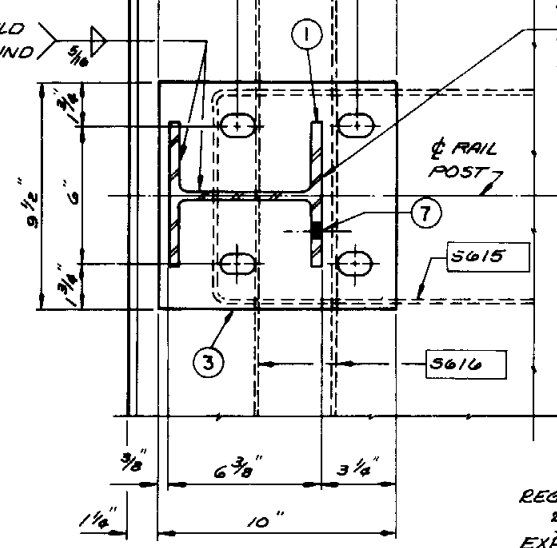
STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS
STRUCTURE B-52-822
 Const. Spec. 1981 Drawn By R.J.G. Plans Checked J.W.
DECK DETAILS SHEET 3 OF 4
X68623



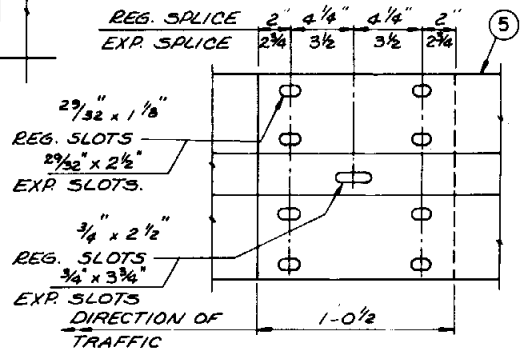
SECTION THRU RAILING



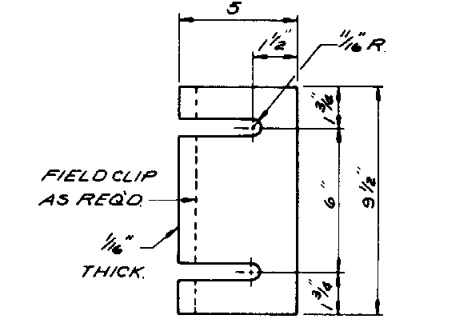
CHANNEL MEMBER DETAILS



**SECTION A
(NO. 5 NOT SHOWN)**

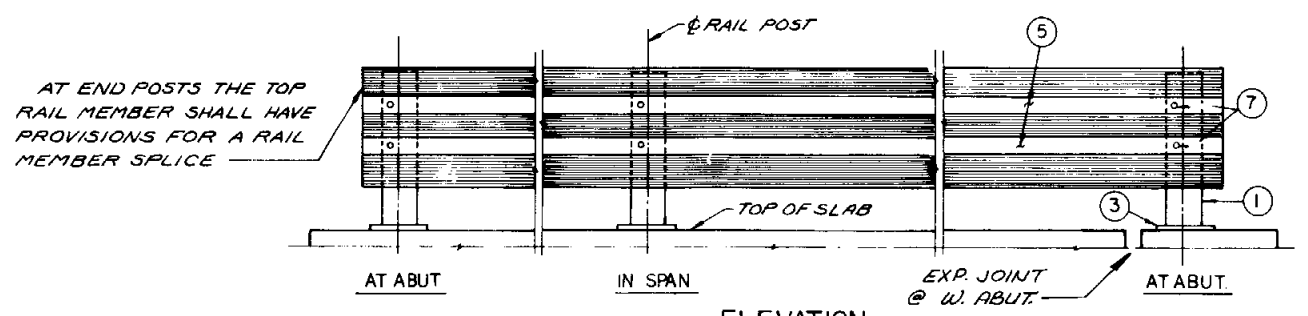


RAIL MEMBER SPLICE
3/8" Ø BUTTON HEAD OVAL SHOULDER BOLTS WITH HEX. NUTS AT ALL SLOTS.

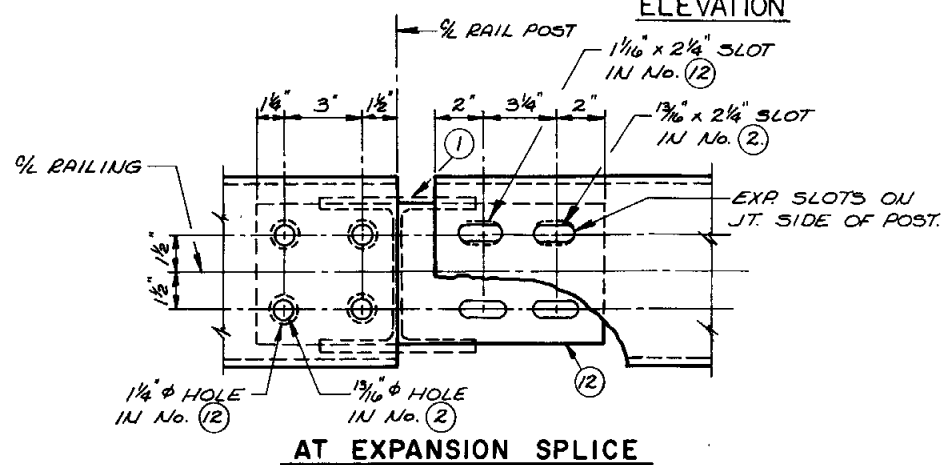


POST SHIMS
4 PER POST

NOTE: EXPANSION SPLICE IN BEAM GUARD AND CHANNEL RAIL IS REQ'D. AT A POST ON EITHER SIDE OF THE EXP. JOINT.



ELEVATION



AT EXPANSION SPLICE

LEGEND

- ① W6 x 25 WITH 2-3/4" x 2 1/2" VERTICAL SLOTS ON ONE SIDE OF POST FLANGE FOR BOLT NO. 7. CUT BOTTOM OF POST TO ANGLE SHOWN. SET POST NORMAL TO GRADE.
 - ② C8 x 11.5, WITH 1 3/16" Ø HOLES, ATTACH TO NO. 4 WITH BOLTS NO. 8. ATTACH CONTINUOUSLY TO A MIN. OF FOUR POSTS AND A MAX. OF EIGHT POSTS.
 - ③ PLATE 1'-9 1/2" x 0'-10", WITH 1 1/16" x 1 1/2" SLOTTED HOLES FOR BOLTS NO. 9. WELD TO NO. 1 AS SHOWN.
 - ④ PLATE 1/2" x 5 3/4" x 0'-6", WITH 1 1/4" Ø HOLE FOR BOLTS NO. 8 WELD TO NO. 1 AS SHOWN.
 - ⑤ CORRUGATED SHEET BEAM, CONFORMING TO A.A.S.H.T.O. DESIGNATION M180-CLASS A, TYPE 2.
- 'THRIE' GUARD RAIL OR EQUAL, MAY BE USED IN LIEU OF THE DOUBLE UNIT PLATE BEAM SHOWN. ATTACH TO NO. 1 WITH BOLTS NO. 7.
- ⑥ 1 3/4" x 3" MOUNTING BOLT WASHER. EIGHT GALVANIZED 2 PER POST REQ'D.
 - ⑦ 7/8" Ø BUTTON HEAD RAIL MOUNTING BOLT WITH ROUND WASHER AND NUT. 2 PER POST REQ'D.
 - ⑧ 5/8" Ø x 2" LG. HEX. BOLTS WITH NUT & TWO WASHERS EACH. 4 REQ'D. PER POST CONNECTION, 8 REQ'D. PER SPLICE.
 - ⑨ A-449 ANCHOR BAR 7/8" Ø x 8 1/2" (MIN. YIELD OF 92 KSI AND ELONGATION OF 18%) WITH A-325 NUT AND WASHER. 4 REQ'D. PER POST. THREAD 3" AND PLACE NORMAL TO PLATE NO. 3.
 - ⑩ BARS 3/4" Ø x 0'-8" LONG. WELD TO BAR NO. 9.
 - ⑪ PLATE 1/2" x 5 3/4" x 0'-11 1/2", WITH 1 1/4" Ø HOLES FOR BOLTS NO. 8 WELD THE SAME AS NO. 4. USE AT CHANNEL FIELD SPLICE ONLY.
 - ⑫ PLATE 1/2" x 5 3/4" x 1'-2 1/2", WITH 1 1/4" Ø HOLES & SLOTTED HOLES AS SHOWN. WELD TO NO. 1 AS SHOWN.

GENERAL NOTES

POST BASE PLATE NO. 3 SHALL BE FLAT WITH ALL SURFACES SMOOTH & FREE FROM WARP. ALL EDGES SMOOTH, STRAIGHT & VERTICAL. ALL PLATE CUTS SHALL BE MACHINE OR MACHINE FLAME CUTS.

ALL MEMBERS, INCLUDING UPPER 4 INCHES OF NO. 9, SHALL BE GALVANIZED AFTER FABRICATION.

FILL BOLT SLOT OPENINGS IN SHIMS & PLATE NO. 3 WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER.

RAIL MEMBERS SHALL BE LAPPED IN THE DIRECTION OF TRAFFIC & THE UPPER RAIL SHALL LAP THE LOWER RAIL.

ALL MATERIAL USED IN FABRICATION SHALL BE MADE FROM MATERIALS CONFORMING TO A.S.T.M. DESIGNATION A36 UNLESS NOTED OTHERWISE.

BID ITEM SHALL BE "STEEL RAILING TYPE 'W'" WHICH SHALL INCLUDE ALL ITEMS BETWEEN LONGIT. LIMITS OF NO. 5 SHOWN IN ELEVATION.

SHIM PLATES 6" x 1 1/2" x 6" MAY BE USED BETH. TOP OF POST AND CHANNEL MEMBER TO ACHIEVE VERT. ALIGNMENT.

AT EXPANSION SPLICES IN RAIL AND CHANNEL MEMBERS, TIGHTEN BOLTS, BACK OFF ONE HALF TURN AND BURR THREADS.

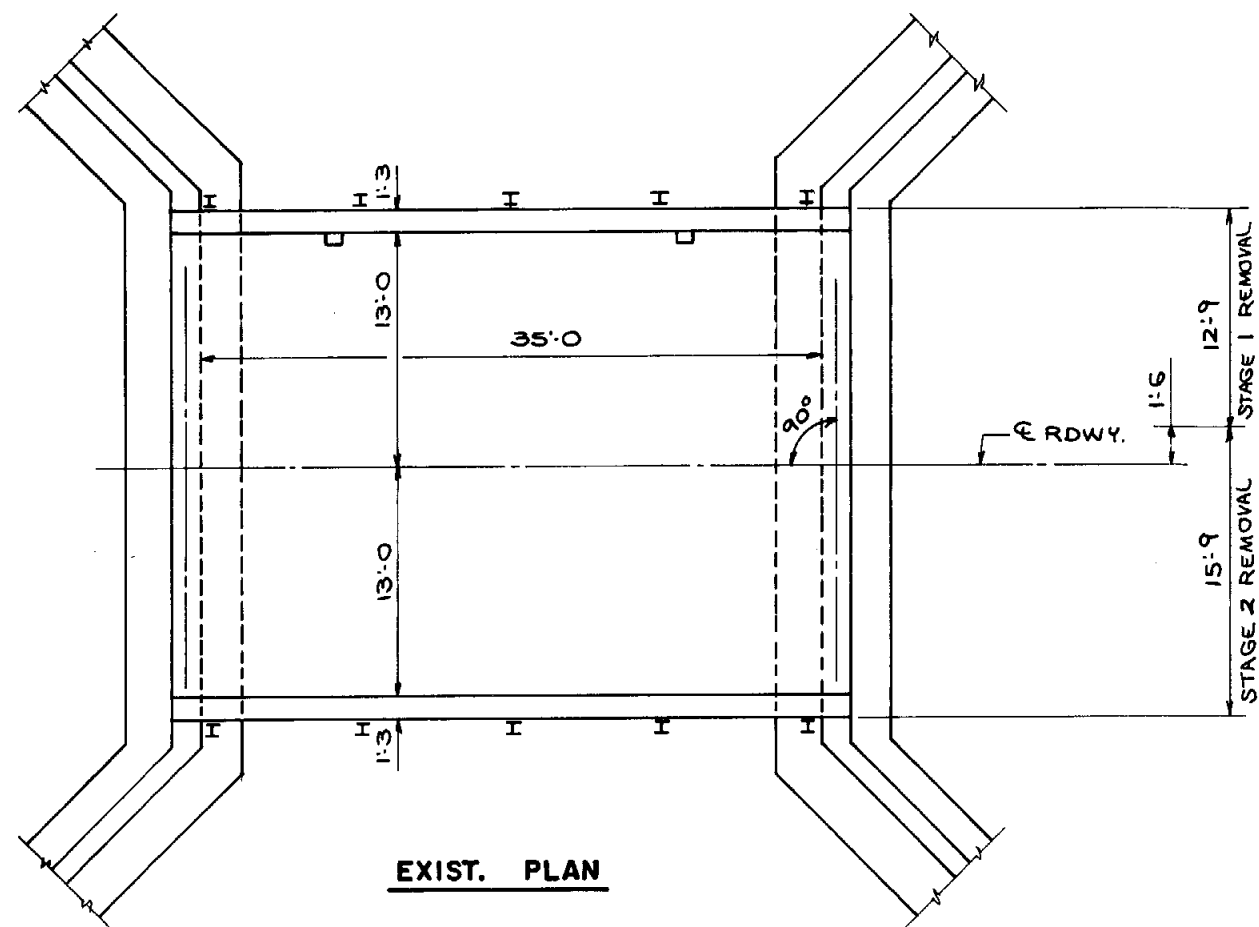
▲ OR MATERIAL OF EQUIVALENT YIELD STRENGTH AND ELONGATION

87°-42'-34" AT SOUTH RAIL
92°-17'-26" AT NORTH RAIL

No.	Date	Revision	By
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS			
STRUCTURE B-52-822			
Const. Spec.	1981	Drawn By R. J. G.	Plans Checked B. W.
STEEL RAILING TYPE 'W'			SHEET 4 OF 4
			X68624

BENCH MARK
 KIEL MARK N.W. CORNER
 ABUT. TOP EL. 949.20

GENERAL NOTES
 DRAWINGS SHALL NOT BE SCALED.
 BAR STEEL REINFORCEMENT SHALL
 BE EMBEDDED 2" CL. UNLESS SHOWN
 OR NOTED OTHERWISE.
 ALL DIMENSIONS ARE
 BASED ON ORIGINAL PLANS.



EXIST. PLAN

DESIGN DATA

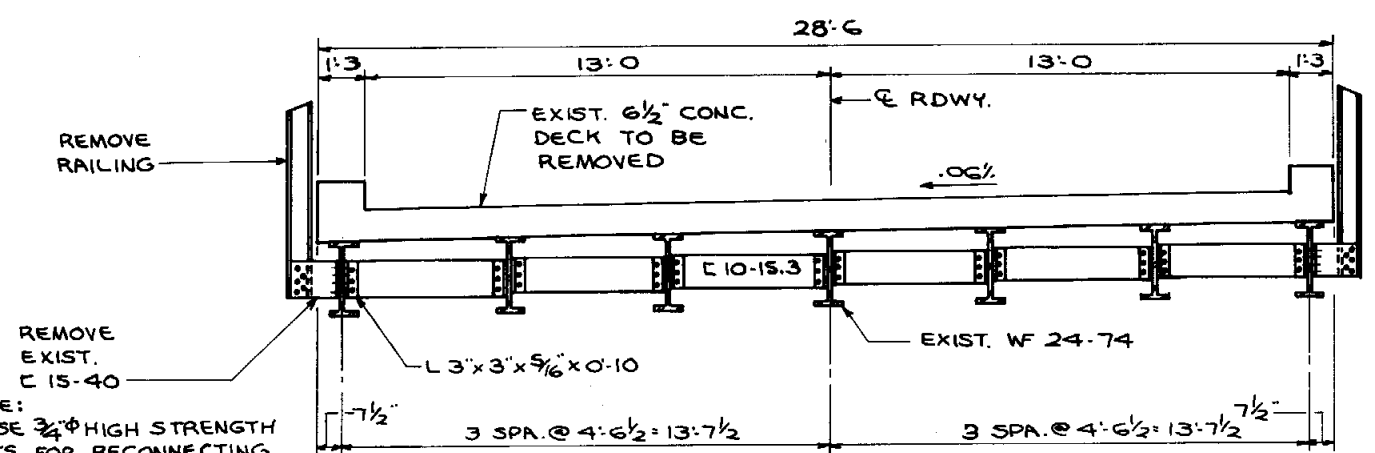
LIVE LOAD:
 DESIGN RATING ——— HS 24
 INVENTORY RATING ——— HS 25
 OPERATIONAL RATING ——— HS 39

ALLOWABLE DESIGN STRESSES:
 CONCRETE MASONRY, SLAB — $f_c = 4,000$ P.S.I.
 ALL OTHER — $f_c = 3,500$ P.S.I.
 BAR STEEL REINFORCEMENT
 GRADE 60 — $f_y = 60,000$ P.S.I.

STRUCTURE IS DESIGNED FOR A FUTURE
 WEARING SURFACE OF 20 LBS. PER SQ. FT.
 STRUCTURAL CARBON STEEL (A.S.T.M. A36)
 $f_s = 20,000$ P.S.I.

LIST OF DRAWINGS

- | | | |
|----------------------------|---|-------|
| 1. DECK REPLACEMENT | X | 68487 |
| 2. SUPERSTRUCTURE | X | 68488 |
| 3. DETAILS | X | 68489 |
| 4. ABUTMENTS | X | 68490 |
| 5. ABUTMENT DETAILS | X | 68491 |
| 6. STEEL RAILING, TYPE 'W' | X | 68492 |

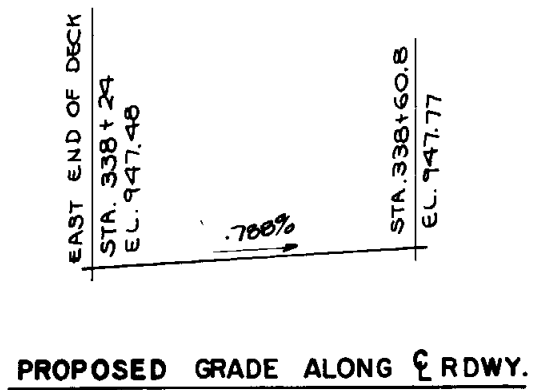


EXIST. CROSS SECTION THRU RDWY.
 (LOOKING WEST)

TOTAL ESTIMATED QUANTITIES

BID ITEM	QTY	UNIT
REMOVING OLD BRIDGE	1	L.S.
CONCRETE MASONRY	54	C.Y.
HIGH STRENGTH BAR STEEL REINFORCEMENT	4,720	LB.
WELDED SHEAR CONNECTORS	582	EA.
STRUCTURAL CARBON STEEL	8,320	LB.
BEARING PADS, ELASTOMERIC	4	S.F.
STEEL RAILING, TYPE 'W'	1	L.S.
PIPE UNDERDRAIN 6"φ	66	L.F.
COATED HIGH STRENGTH BAR STEEL REINF.	3,900	LB.
CONCRETE SURFACE REPAIR	52	S.F.
EXCAVATION FOR STRUCTURES, BRIDGES B-52-823	1	L.S.
NON-BID ITEM		
FILLER	1/2"	SIZE

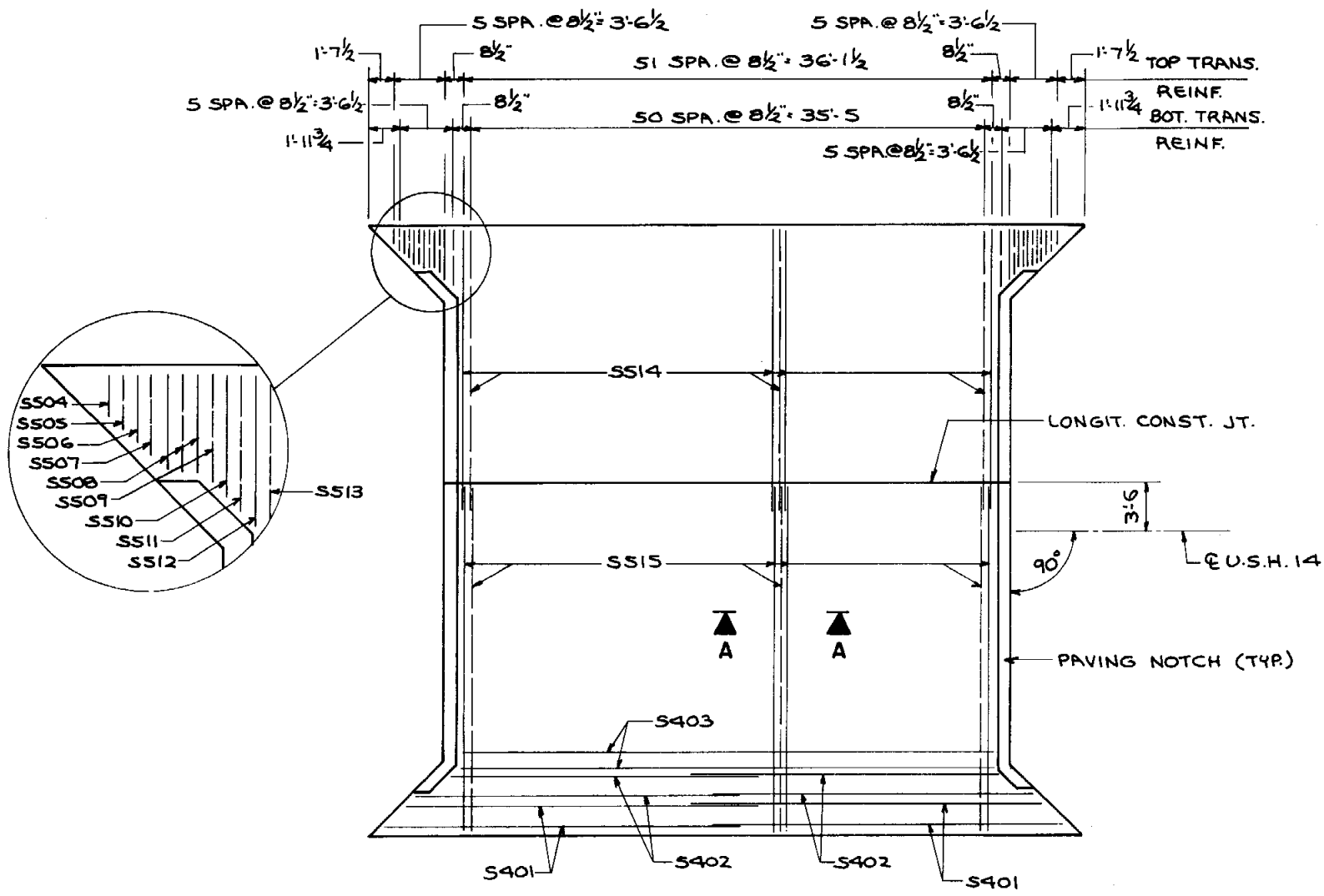
NOTE:
 USE 3/4"φ HIGH STRENGTH
 BOLTS FOR RECONNECTING
 DIAPH. AT ALL OTHER LOCATIONS
 FILL HOLES WITH FILL BOLTS.



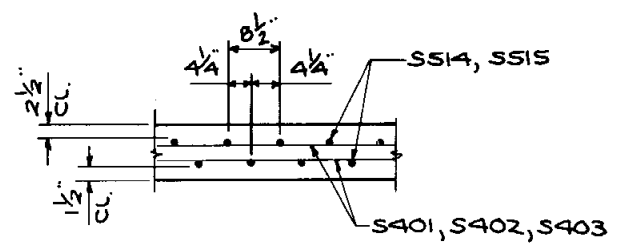
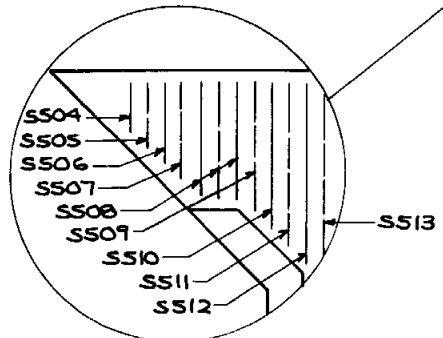
PROPOSED GRADE ALONG RDWY.

LAYOUT

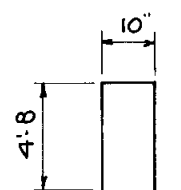
No.	Date	Revision	By
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-52-823			
U.S.H. 14 OVER W.B.R. MILL CREEK			
County	RICHLAND	Town	SYLVAN
Design Spec.	A.A.S.H.T.O. '81	Load	HS 24
Designed By	C. RAY	Checked	KAB
Drawn	DB	Placed	B.M.
By	Stanley W. Woods	Date	5/25/82
Chief Bridge Engineer			
DECK REPLACEMENT			SHEET 1 OF 6 X 68487



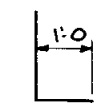
PLAN SHOWING BAR STEEL REINF.



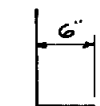
SECTION A



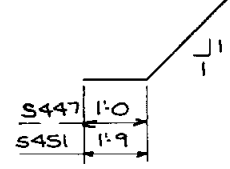
S 617



S 446



S 448



S 447,
S 451

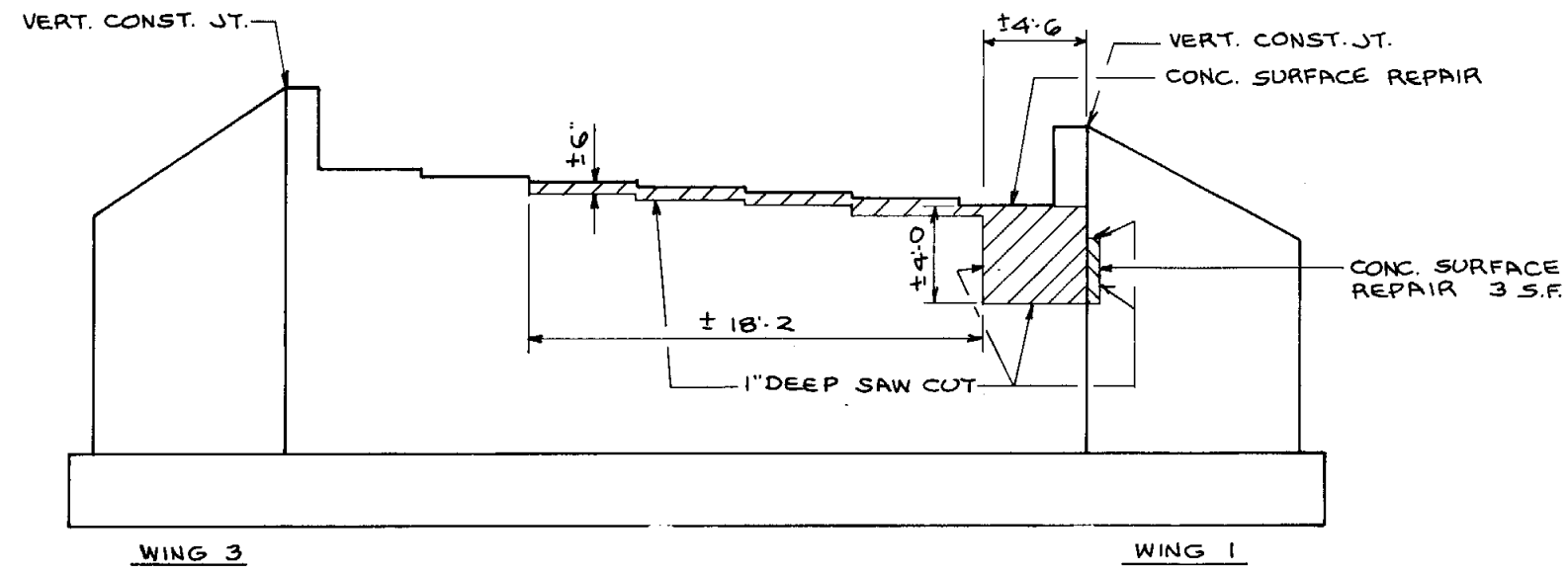
NOTE:
COATED BARS TO BE
PLACED IN TOP MAT
OF BARS IN SLAB.

- 1 ■ HALF OF THESE BARS TO BE COATED.
- 2 ■ 36 " " " " " "
- 3 ■ ALL " " " " " "
- 4 ■ 8 " " " " " "
- 5 ■ S2 " " " " " "

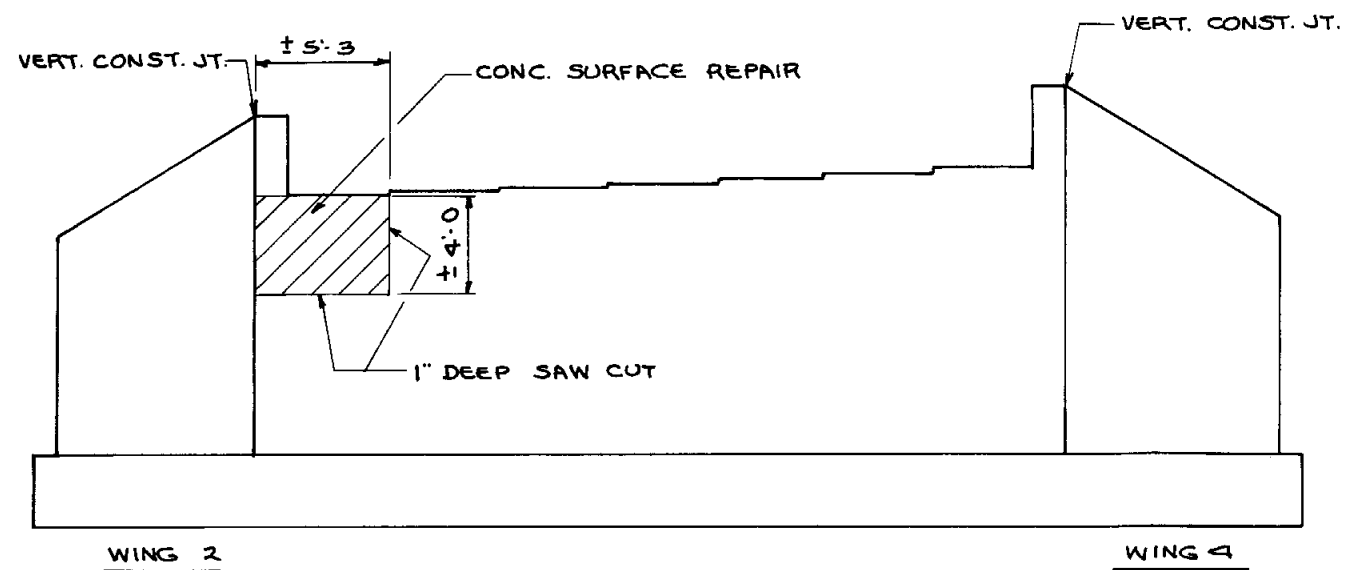
BILL OF BARS				
MARK	NO.	LENGTH	BENT	LOCATION
1 ■	S401	16	23-8	SLAB-LONGIT.-TOP & BOT.
1 ■	S402	16	21-11	" " " "
2 ■	S403	80	36-1	" " " "
3 ■	SS04	4	1-1	" - TRANS.-TOP
	SS05	4	1-6	" " - BOT.
3 ■	SS06	4	1-10	" " - TOP
	SS07	4	2-2	" " - BOT.
4 ■	SS08	12	2-7	" " - TOP & BOT.
	SS09	4	2-10	" " - BOT.
3 ■	SS10	4	3-2	" " - TOP
	SS11	4	3-5	" " - BOT.
3 ■	SS12	4	3-11	" " - TOP
	SS13	4	4-2	" " - BOT.
5 ■	SS14	103	18-8	" " - TOP & BOT.
5 ■	SS15	103	23-7	" " " "
3 ■	S616	32	4-0	" AT RAIL POSTS
3 ■	S617	16	10-0	" " " "
	S418	4	3-8	ABUTMENTS AT WINGS 1 & 2
	S419	4	4-5	" " " " 1 & 2
	S420	4	4-11	" " " " 1 & 2
	S421	4	4-7	" " " " 1 & 2
	S422	24	3-10	" " " " 1 & 2
	S423	4	10-3	ABUTMENTS AT WINGS 1 & 2
	S424	4	1-1	" " " " "
	S425	4	3-6	" " " " "
	S426	8	9-3	" " " " "
	S427	4	8-6	" " " " "
	S428	4	6-2	" " " " "
	S429	4	3-9	" " " " "
	S430	4	3-0	" " " " "
	S431	4	4-8	ABUTMENTS AT WINGS 3 & 4
	S432	4	5-0	" " " " "
	S433	4	5-6	" " " " "
	S434	4	5-5	" " " " "
	S435	4	12-6	" " " " "
	S436	4	2-1	" " " " "
	S437	4	4-9	" " " " "
	S438	4	9-0	" " " " "
	S439	4	10-0	" " " " "
	S440	4	9-9	" " " " "
	S441	4	7-3	" " " " "
	S442	4	5-3	" " " " "
	S443	4	3-9	" " " " "
	S444	4	3-0	" " " " "
	S445	24	3-6	DIAPH. AT ABUTMENTS
	S446	92	3-3	" " " " "
	S447	8	4-0	" " " " "
	S448	32	2-11	" " " " "
	S449	32	4-3	ABUTMENTS AT WINGS 3 & 4
	S450	8	2-0	DIAPH. AT ABUTMENTS
	S451	72	3-6	" " " " "

NOTE:
THE FIRST DIGIT OF A BAR MARK SIGNIFIES
THE BAR SIZE.

No.	Date	Revision	By
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS			
STRUCTURE B-52-823			
Const. Spec	1981	Drawn By	DB
		Plans Checked	B.W.
DETAILS			SHEET 3 OF 6
X 68489			

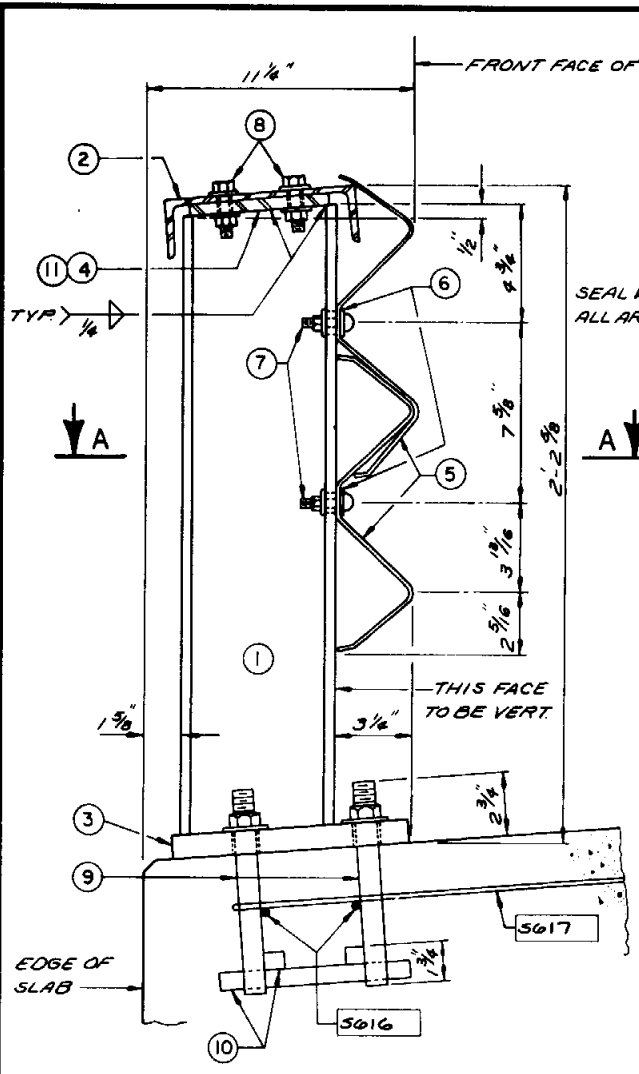


EAST ABUTMENT
 (LOOKING EAST)
 (EXIST. ABUT. SHOWN)

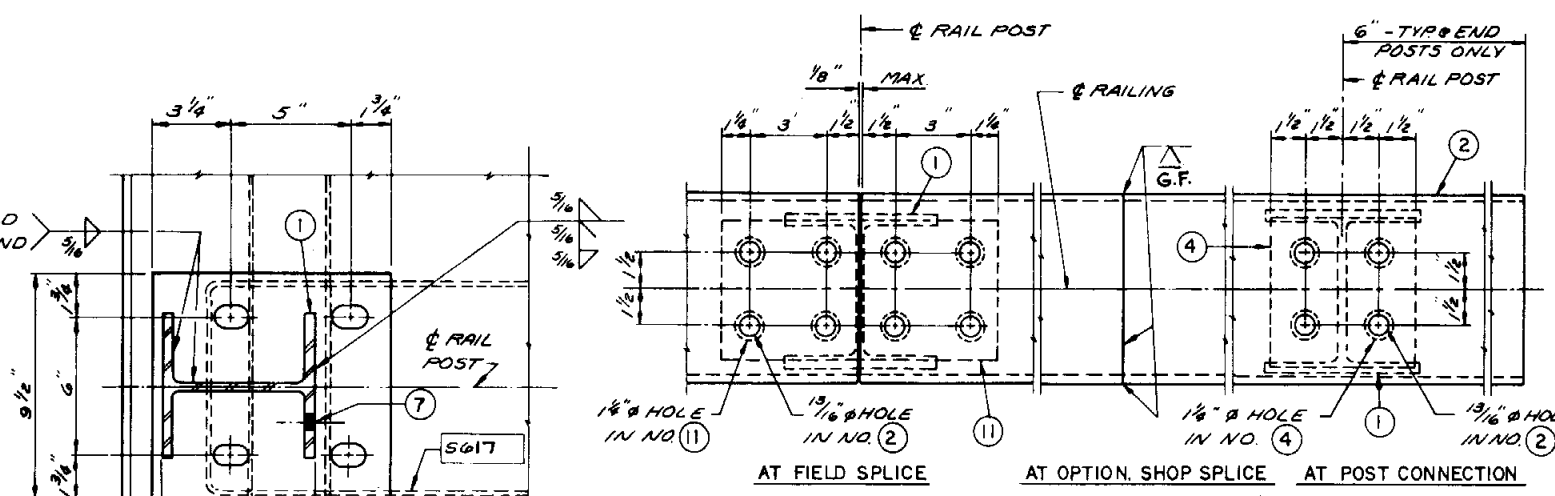


WEST ABUTMENT
 (LOOKING WEST)
 (EXIST. ABUT. SHOWN)

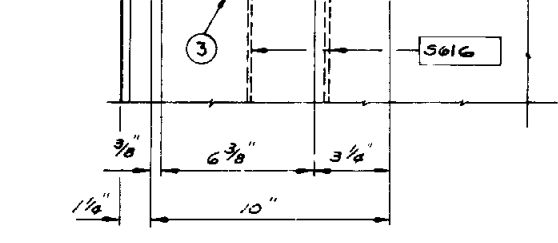
No.	Date	Revision	By
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS			
STRUCTURE B - 52-823			
Const. Spec.	1981	Drawn By	DB
		Plans Checked	B.W.
ABUTMENT DETAILS			SHEET 5 OF 6
X 6849I			



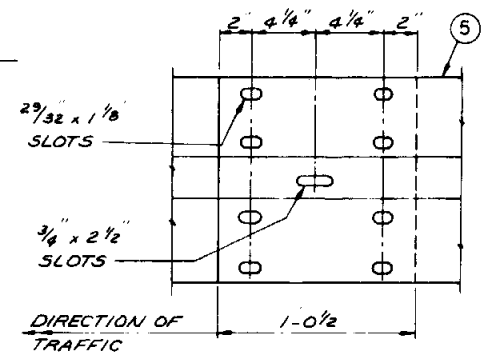
SECTION THRU RAILING



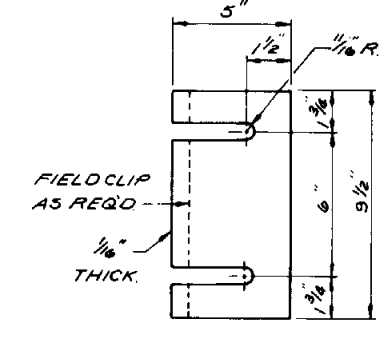
CHANNEL MEMBER DETAILS



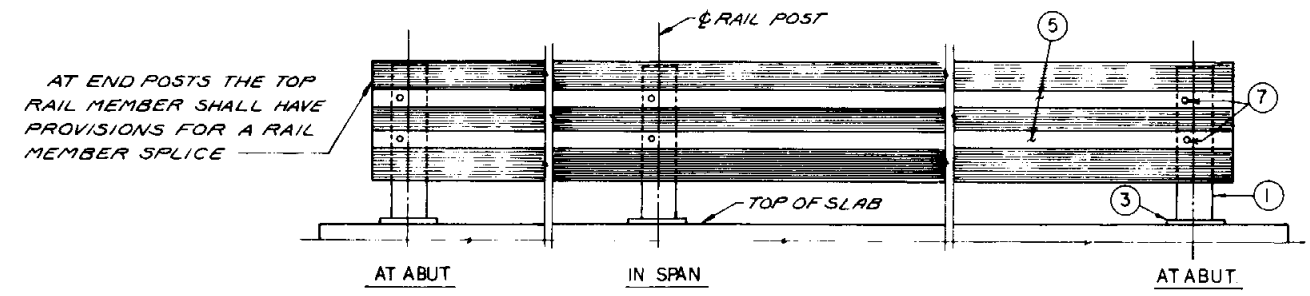
**SECTION A
(NO. 5 NOT SHOWN)**



RAIL MEMBER SPLICE
3/8" Ø BUTTON HEAD OVAL SHOULDER BOLTS WITH HEX. NUTS AT ALL SLOTS.



POST SHIMS
4 PER POST



ELEVATION

LEGEND

- 1 W6x25 WITH 2- 3/4", 2 1/2" VERTICAL SLOTS ON ONE SIDE OF POST FLANGE FOR BOLT NO. 7. CUT BOTTOM OF POST TO MATCH CROSS SLOPE OF RDWY. PLACE POST NORMAL TO GRADE LINE.
- 2 C8x11.5, WITH 1 1/2" Ø HOLES, ATTACH TO NO. 1 WITH BOLTS NO. 8. ATTACH CONTINUOUSLY TO A MIN. OF FOUR POSTS AND A MAX. OF EIGHT POSTS.
- 3 PLATE 1" x 9 1/2" x 0" - 10", WITH 1 1/2" x 1 1/2" SLOTTED HOLES FOR BOLTS NO. 9. WELD TO NO. 1 AS SHOWN.
- 4 PLATE 1/2" x 5 3/4" x 0" - 6", WITH 1 1/2" Ø HOLE FOR BOLTS NO. 8 WELD TO NO. 1 AS SHOWN.
- 5 CORRUGATED SHEET BEAM, CONFORMING TO A.A.S.H.T.O. DESIGNATION M180-CLASS A, TYPE 2.
- 'THRIE' GUARD RAIL OR EQUAL, MAY BE USED IN LIEU OF THE DOUBLE UNIT PLATE BEAM SHOWN. ATTACH TO NO. 1 WITH BOLTS NO. 7.
- 6 1 3/4" x 3" MOUNTING BOLT WASHER. EIGHT GAUGE GALVANIZED. 2 PER POST REQ'D.
- 7 3/8" Ø BUTTON HEAD RAIL MOUNTING BOLT WITH ROUND WASHER AND NUT. 2 PER POST REQ'D.
- 8 3/8" x 2" 16. HEX. BOLTS WITH NUT & TWO WASHERS EACH. 4 REQ'D. PER POST CONNECTION, 8 REQ'D. PER SPLICE.
- 9 A-449 ANCHOR BAR 7/8" Ø x 3 1/2" (MIN. YIELD OF 92 KSI AND ELONGATION OF 14%) WITH A-325 NUT AND WASHER. 4 REQ'D. PER POST. THREAD 3" AND PLACE NORMAL TO PLATE NO. 3.
- 10 BARS 3/4" Ø x 0" - 8" LONG WELD TO BAR NO. 9.
- 11 PLATE 1/2" x 5 3/4" x 0" - 11 1/2", WITH 1 1/2" Ø HOLES FOR BOLTS NO. 8. WELD THE SAME AS NO. 4. USE AT CHANNEL FIELD SPLICE ONLY.

GENERAL NOTES

POST BASE PLATE NO. 3 SHALL BE FLAT WITH ALL SURFACES SMOOTH & FREE FROM WARP & ALL EDGES SMOOTH, STRAIGHT & VERTICAL ALL PLATE CUTS SHALL BE MACHINE OR MACHINE FLAME CUTS.

ALL MEMBERS, INCLUDING UPPER 4 INCHES OF NO. 9, SHALL BE GALVANIZED AFTER FABRICATION.

FILL BOLT SLOT OPENINGS IN SHIMS & PLATE NO. 3 WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER.

RAIL MEMBERS SHALL BE LAPPED IN THE DIRECTION OF TRAFFIC & THE UPPER RAIL SHALL LAP THE LOWER RAIL.

ALL MATERIAL USED IN FABRICATION SHALL BE MADE FROM MATERIALS CONFORMING TO A.S.T.M. DESIGNATION A36 UNLESS NOTED OTHERWISE.

BID ITEM SHALL BE "STEEL RAILING TYPE 'W'" WHICH SHALL INCLUDE ALL ITEMS BETWEEN LONGIT. LIMITS OF NO. 5 SHOWN IN ELEVATION.

SHIM PLATES 6" x 1/4" x 6" MAY BE USED BETW. TOP OF POST AND CHANNEL MEMBER TO ACHIEVE VERT. ALIGNMENT.

OR MATERIAL OF EQUIVALENT YIELD STRENGTH & ELONGATION.

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STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS			
STRUCTURE B-52-823			
Const. Spec.	1981	Drawn By DB	Plans Checked B.U.
STEEL RAILING TYPE 'W'			SHEET 6 OF 6 X 68492