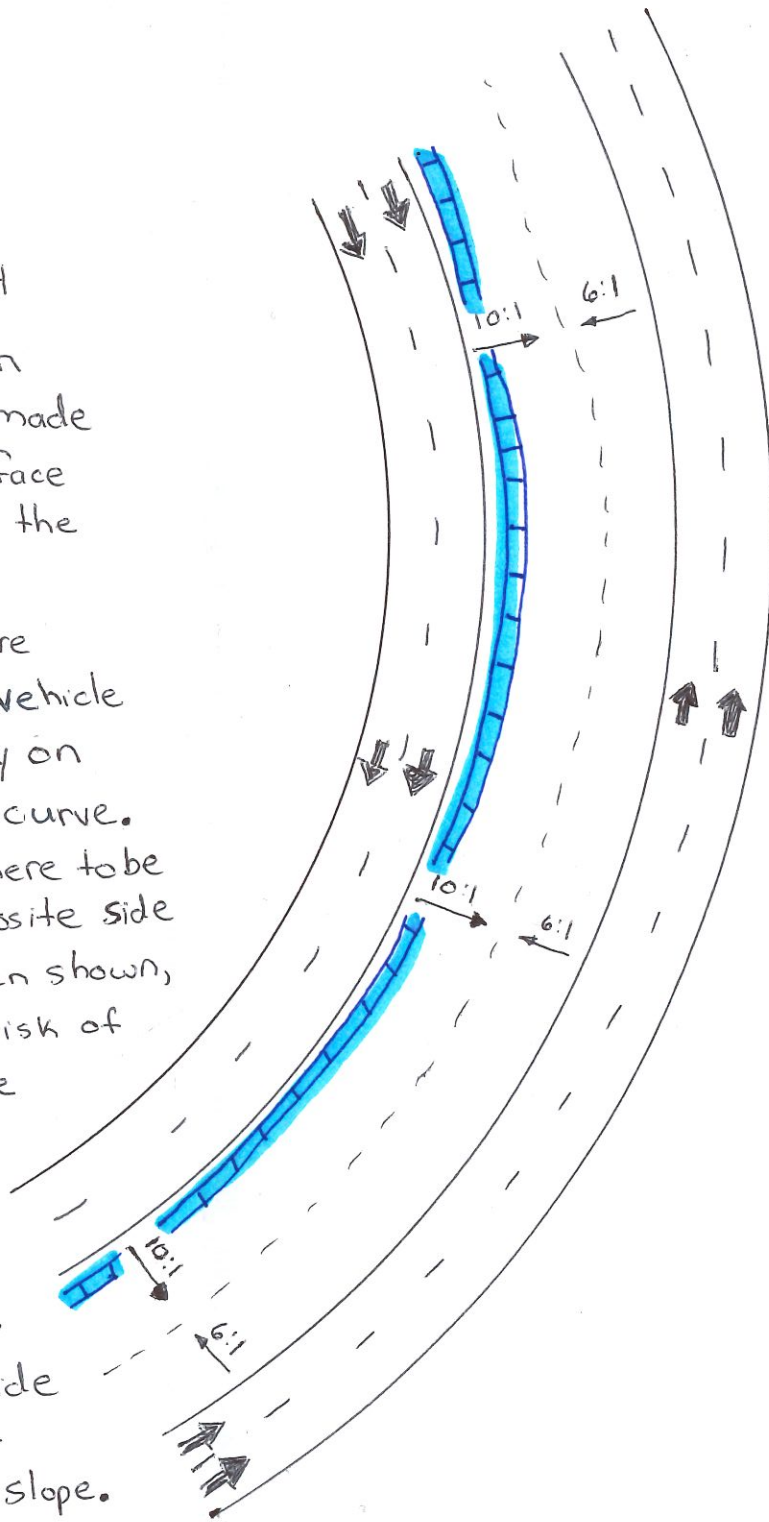



Just to Clarify #4

in your response, an argument could be made to place the double face MGS w-beam along the shallower slope, as shown, because there is more risk of a vehicle leaving the roadway on the outside of the curve. If the barrier were to be placed on the opposite side of the median, then shown, there is a higher risk of a vehicle hitting the double face MGS w-beam from the reverse direction and the vehicle may override or underide the w-beam after traversing up the 6:1 slope. The double face MGS w-beam as shown may be a safer option for an errant vehicle.



 Double Face MGS w-beam